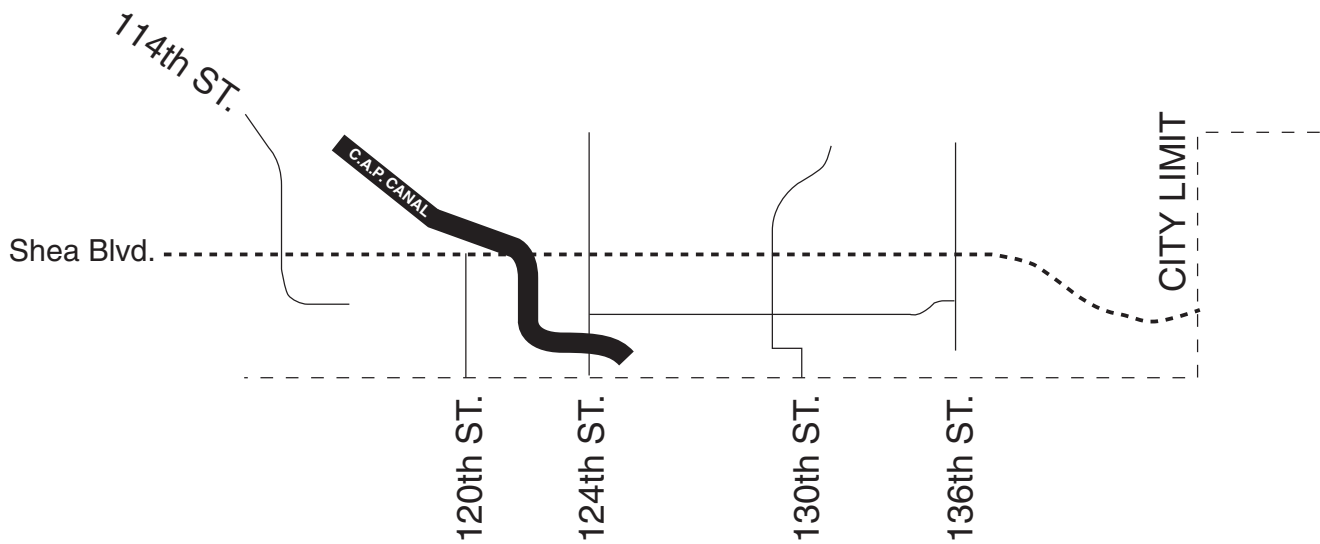


**Shea Boulevard**  
**Streetscape Guidelines**  
1-26-94

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Area to follow Shea  
Streetscape Guidelines .....

FIGURE 1



NOT TO SCALE

## INTRODUCTION

Dramatic mountain and valley views provide the backdrop for the East Shea Area which acts as a gateway to Scottsdale. Shea Boulevard is a regional transportation artery that connects the metropolitan area with Fountain Hills as well as other destinations northeast of the region. The importance of this roadway as a visual resource has been demonstrated in the past through its designation as a Scenic Corridor which requires that open space be designated beyond the public right-of-way. Although the policy has been in existence since the late 1970's, the June 1993 Shea Area Plan reaffirmed the Scenic Corridor Policy and established additional guideline criteria for the width of the Scenic Corridor. In 1992, a task force identified Shea Boulevard as a high priority to receive a concerted effort in the development of a streetscape theme.

As a follow up to the 1993 Shea Area Plan, the Scottsdale City Council directed staff to prepare the document contained herein so that a unified streetscape image is developed. A team of staff members from Planning and Community Development, Transportation Planning, Capital Project Management, and Transportation Maintenance has assisted in the development and review of these concepts.

## APPLICABILITY

To build on and strengthen past philosophies, this document outlines elements that should be addressed by all development projects that front on Shea Boulevard, as well as all city projects within Shea Boulevard Right-of-Way. The guidelines will apply to areas east of the intersection of Frank Lloyd Wright Boulevard and Shea Boulevard as illustrated in Figure 1.

The following design components are included to create an enhanced streetscape appearance:

- 1) Landscape concept for Shea Boulevard and Scenic Corridor
- 2) Boulevard symbol graphic
- 3) Entry monument concept
- 4) Gateway concept
- 5) Wall design
- 6) Light pole color scheme
- 7) Building setbacks/height setbacks
- 8) Sidewalks
- 9) Maintenance
- 10) Implementation

## LANDSCAPE CONCEPT

The Environmental Design Element of the General Plan has identified that plant material for medians and landscaped right-of-way areas should consist of the naturally occurring vegetation. The overall goal is that the scenic corridor and median areas should portray a natural appearance in plant types and densities as shown in figure 2.

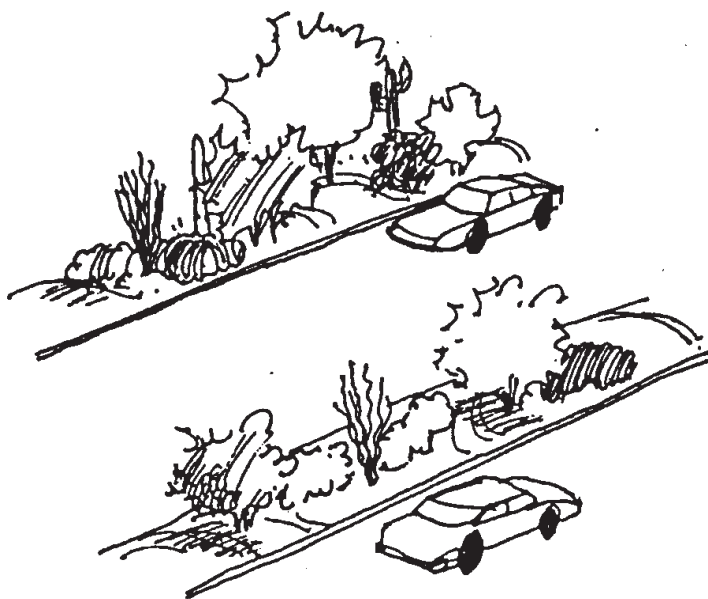


Figure 2

Where revegetation is needed for the scenic corridor and where landscape is proposed in medians or right-of-way, the plant palette should follow these criteria:

1. Plant material should reflect what is currently established in the surrounding landscape or be native plant material. All plant materials should be included in the Arizona Department of Water Resources approved plant list. Plant materials that are desert adapted but exhibit tall growth characteristics or are out of character with the natural landscape should be avoided.
2. Trees to be used as consistent unifying elements include: Foothill Palo Verde (*Cercidium microphyllum*), Velvet Mesquite (*Prosopis velutina*), and Ironwood (*Olneya tesota*).
3. There should be trees planted at a minimum rate of one for every 1,000 square feet of landscaped area.
4. Shrubs should be planted at a minimum rate of one for every 230 square feet of landscaped area. Trees and shrubs may be grouped into appropriate locations.
5. Plant material with overlapping flowering seasons should be used.
6. Ground covers and native seed mixes are encouraged for planting between trees and shrubs.
7. Succulents should be planted at the following rates: Saguaro, Barrel Cactus, and Ocotillo; 1 per 5,000 square feet. Cholla, Prickly Pear, and hedgehog; 1 per 250 square feet.
8. Where streets intersect with Shea Boulevard, proper sight lines should be preserved. Conflicts between these guidelines and needed sight distances should be evaluated on a case by case basis with safety being a priority. Refer to City of Scottsdale's Street Geometrics for further information on sight line requirements.

In conjunction with the widening plans for Shea between 124th and 136th Streets, plant material will be installed in the medians and hydroseed applications will be installed within the right-of-way. After the plant material that was started from seed has been established for at least two growing seasons. An assessment should be made to determine if supplemental plantings are necessary to augment these areas. The conceptual landscape design is illustrated in figure 3.

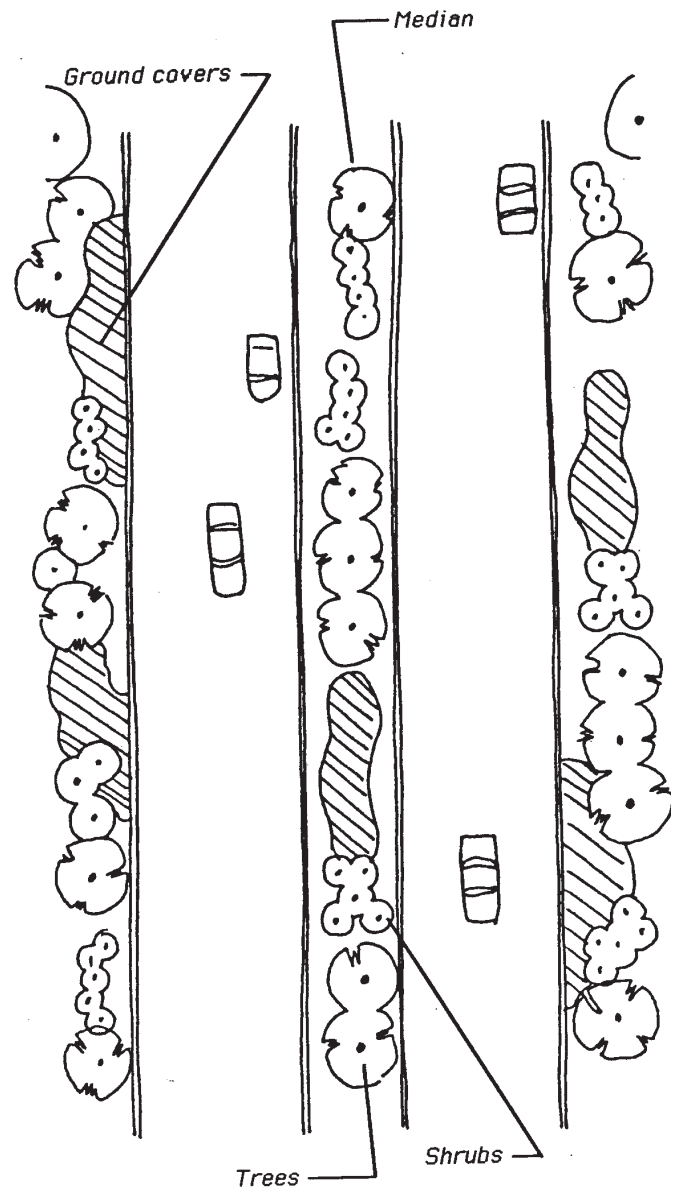


Figure 3

## BOULEVARD SYMBOL GRAPHICS

A sense of identity for the Shea Boulevard area can be achieved through a common graphic or symbol that is used repeatedly throughout the boulevard. For this purpose, the graphic in figure 4 can be used in a variety of applications such as railing, wall imprints, signs and other graphic details.

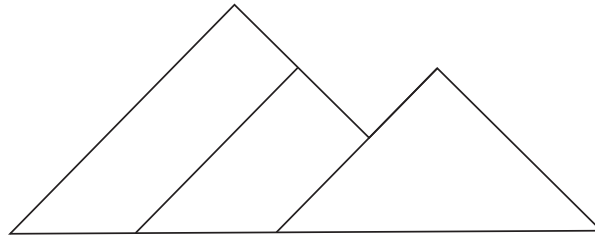


Figure 4

## ENTRY MONUMENT CONCEPT

To reinforce the sense of entry into Scottsdale, an entry monument should be located at the east city limit line along the north side of Shea Boulevard and should be built along with the Shea Boulevard roadway improvements or as a separate effort. The standard city entry monument design is shown in figure 5 and should include an additional planter wall with the Boulevard symbol graphic incorporated as part of a reveal, railing or other accent feature.

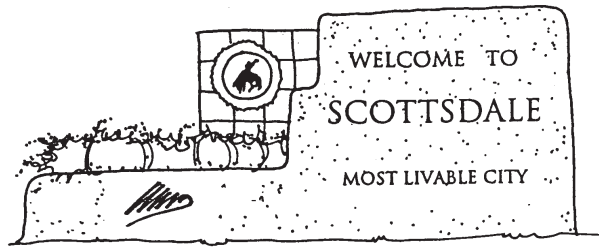


Figure 5

## GATEWAY CONCEPT

In addition to an entry monument, the area of Shea Boulevard near the east city limit line lends itself well to the inclusion of a gateway feature. The gateway concept originally was recommended by a task force during the ARTSCAPE project in 1990. This effort was undertaken to explore potential areas where public art should be located. In their findings, the task force recommended a gateway "that acknowledges the spectacular views of the city and mountains as motorists sweep down the hill into Scottsdale..." The gateway could include various elements such as sculpture, a scenic overlook, or a cultural/historical point of interest. An example of this type of project occurs at the McDowell Road and 64th Street intersection where the historic significance of the canals, desert heritage, and the summer solstice is presented in an environmental public art format. This example is not intended to specifically spell out what the gateway feature should be but is an idea of how to approach a potential theme. The following criteria should be considered for the gateway feature:

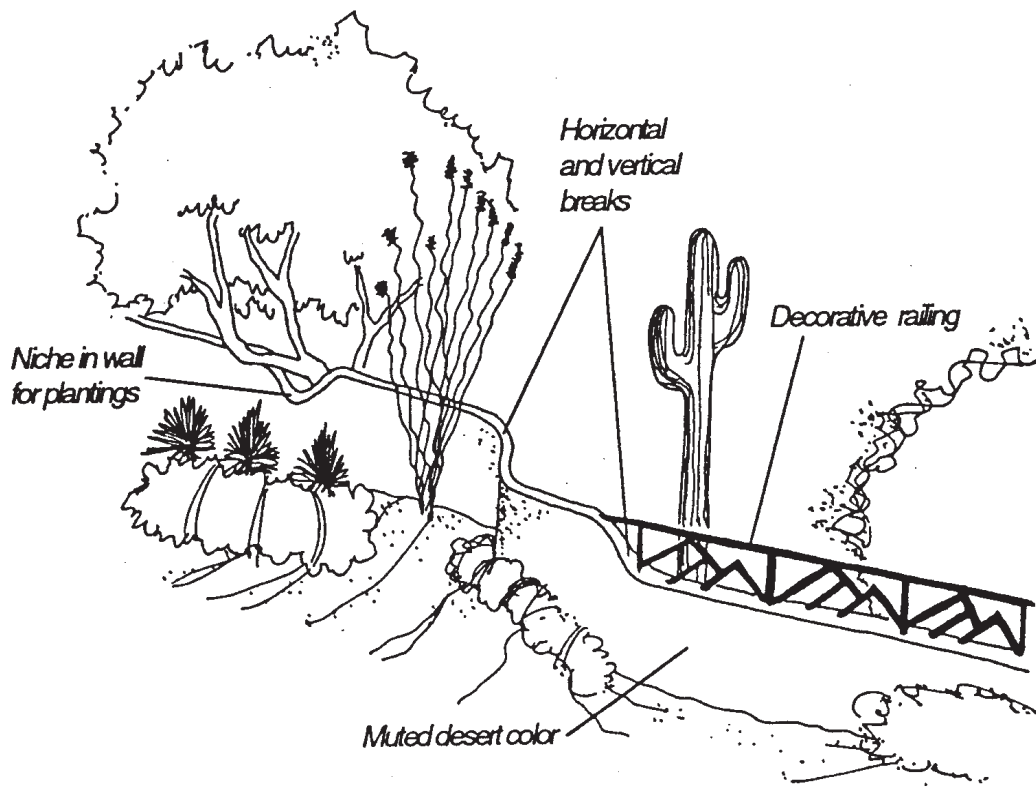
- 1) Additional funding of approximately \$150,000 should be appropriated for a gateway to be built in conjunction with the Shea Boulevard widening improvements from 136th to 144th Streets.
- 2) Be positioned to appropriately depict a sense of entry into the city
- 3) Be the result of a competitive process from a field of artists with experience in outdoor applications.

## WALL DESIGN

Wherever possible, development perimeter walls should be avoided so that views to the natural environment can be enhanced. In the event that walls are necessary, special attention should be given to ensure that the wall reflects the natural surroundings. These are general guidelines that could be incorporated in the wall design:

1. Muted desert tones (as per ESLO design guidelines) and undulations in the wall are encouraged.
2. Vertical and horizontal breaks in wall alignment to minimize height and mass of the wall
3. Special niches for plant material to be integrated into the wall.

The following illustration is an example of design elements that should be incorporated when considering wall construction.



## LIGHT POLE COLOR SCHEME

The lush desert vegetation provides the basis for which a light pole color is selected. The following are potential colors that have green/gray tints. These should be tested prior to selecting of one for painting on all traffic signal, street light, and power poles east of 114th Street (not including the 114th Street intersection). This effort should be handled in conjunction with Shea Boulevard widening improvements.

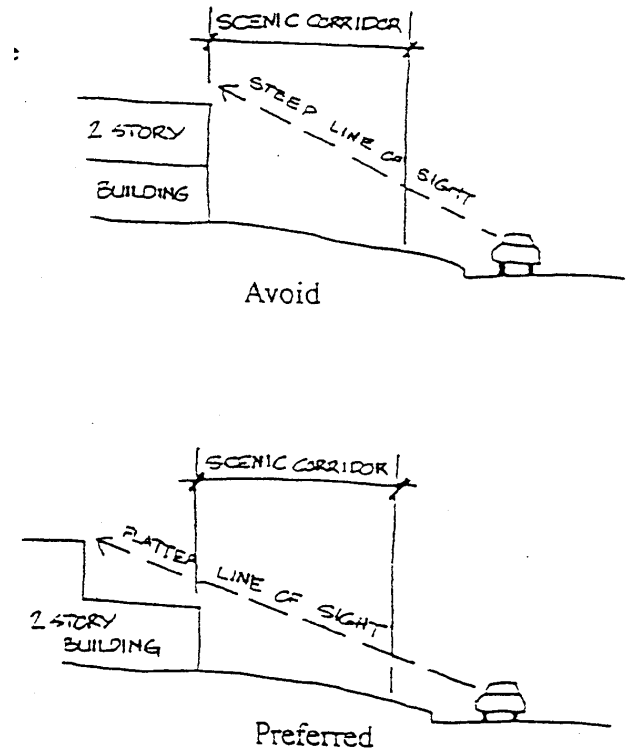
Frazer or approved equal:

- 5543- Cicada
- 4923- Riverbend
- 4913- Stratford

## BUILDING HEIGHT/SETBACKS

Much of the scenic quality of the Shea area can be attributed to well preserved views of the mountains and valley. This makes it vitally important that new development provide adequate setbacks from the scenic corridor and provide innovative solutions for buildings where more than one story are proposed. The overall effect on views should be judged as to whether the building can be built with two or more stories and retain an adequate level of view corridors. To this end, these guidelines should be used:

1. Where more than one story is proposed, subsequent stories should be stepped back from the first. This also applies to areas where the building is within 100 feet of a wash or vista corridor that intersects with Shea Boulevard.
2. Setback distances from the scenic corridor should increase as building height increases.
3. Where retaining walls are necessary, they should be stepped back in a fashion similar to how buildings are stepped back.



## SIDEWALKS

A sidewalk should be provided on both sides of the street and set back from the curb as far as possible with returns to the curb only at intersections and driveways. A sidewalk will be built on the north side of Shea along with the 124th Street to 136th Street widening improvements by the City; a sidewalk on the south side of Shea should also be constructed as development occurs.

The Shea Boulevard Transportation/Access Policy has indicated that a 10' sidewalk be the preferred dimension for a walk on the south side of Shea.

## **MAINTENANCE**

Responsibility for maintenance of landscape improvements behind the curb, both sides of Shea, lies with the property owner or the assigned maintenance organization. The City will provide maintenance for medians.

## **IMPLEMENTATION**

The majority of the area that is affected by these guidelines is undeveloped at this time. The portions that are developed and have walls, sidewalks and landscape in place should not be required to undergo a retrofit program to comply with these guidelines. New development projects will be required to implement these guidelines within their area of responsibility for the Shea frontage. The City will be responsible for addressing the following items in addition to determining whether their inclusion in a capital improvement project is necessary.

1. Installation of median landscape (currently funded with 124th- 136th St. widening):
2. Assessing the areas which received hydroseed applications between 124th and 136th Street for future revegetation projects if necessary. To be completed not sooner than after two growing seasons.
3. Entry monument signage.
4. Artist designed gateway feature.
5. Coordinate the selection of a paint color to be used on signal, street light and power poles as part of the Shea Boulevard widening.
6. As development occurs on the south side of Shea Boulevard, a 10' sidewalk should be installed with other streetscape improvements as part of a development project. The city should assess this area in the future to determine if a capital project would be necessary to build the sidewalk.